

3.13 PEMBERTON AVENUE

OVERVIEW

GENERAL

Pemberton Avenue is a standard (66 ft / 20.11 m) public road right of way and designated a collector road for vehicles and active transportation. Zoning Bylaw Regulation 4.42 requires a 1.5 m building setback along any property fronting Pemberton Avenue to support sufficient space for separated bike lanes and one side of parking or a turning lane. This streetscape standard shows the Ultimate design (75.45 ft / 23 m) where both sides of the street have a 1.5 m building setbacks. The streetscape design is slightly different if there is a parking lane versus a turning lane. Both are shown on the plan view but only one design should be applied per block. In all cases, a 1.5 m Public Right of Way will be required to use the additional space for public sidewalk use. Pemberton Avenue is angled differently to other streets Downtown making intersection design more complex. The intersection designs are general only and will require further detailed civil design and review.

USE

The primary use of Pemberton Avenue is for safe, separated, pleasant, and efficient cycling movement and collector vehicle movement. Secondary use is for pedestrian movement, on street -parking on one side of the road (where turning lanes are not needed) and commercial activation with wide sidewalks and some space for commercial focused streetscape furnishings (seating, bike racks, waste receptacles). Drive aisles should be designed wide enough to support current or future public transit use. Most of Pemberton Avenue is a main fire route to be designed with emergency vehicles and snow removal considerations.

LANDSCAPING

Pemberton Avenue should support a rhythmic planting of street trees in tree grates. Clumping of street trees and or planter beds may not be appropriate for the complexity of this street and should only be considered where appropriate. Small sections of planting for narrow and low maintenance grasses or pollinators can be explored if space permits along the buffered bike lane, only when the buffer is not adjacent to parking. See Street Tree and Planting Guidelines for more details.

PEMBERTON AVENUE

OVERVIEW

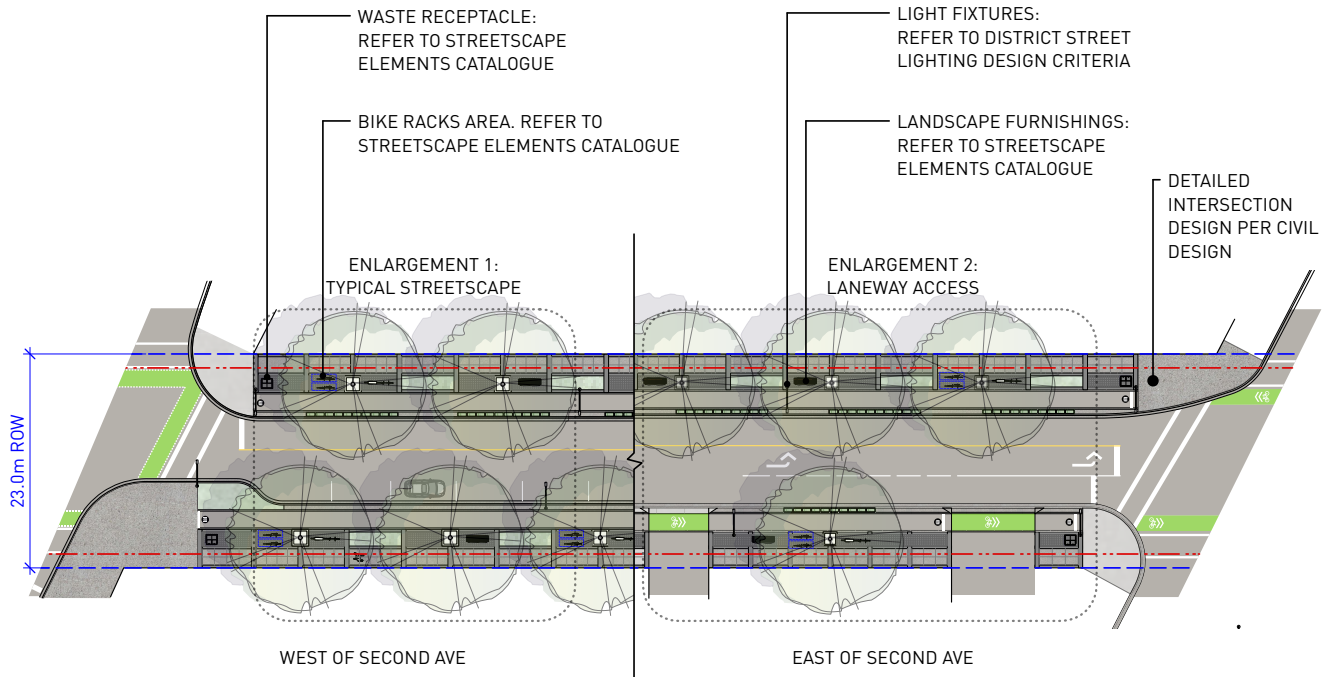
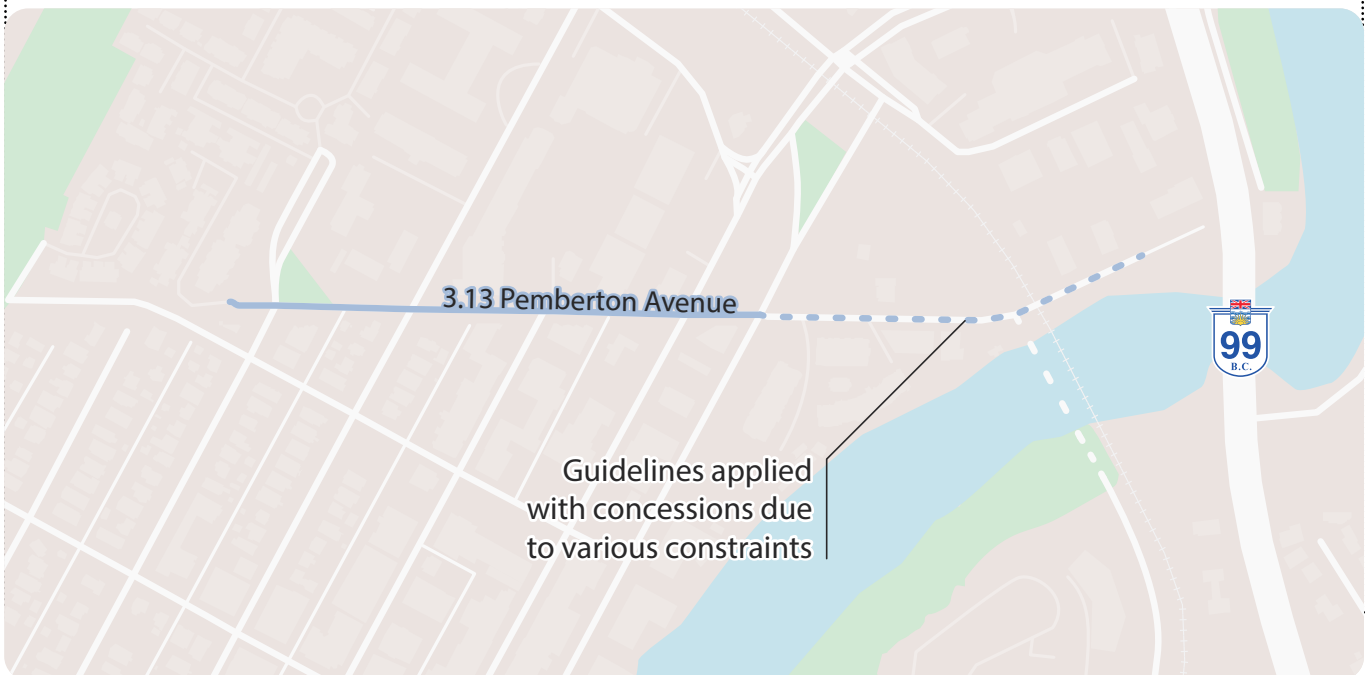
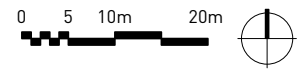


FIG. 1 PEMBERTON AVE - OVERVIEW
Scale: 1:700



PEMBERTON AVENUE

TYPICAL STREETSCAPE – WEST OF SECOND AVENUE

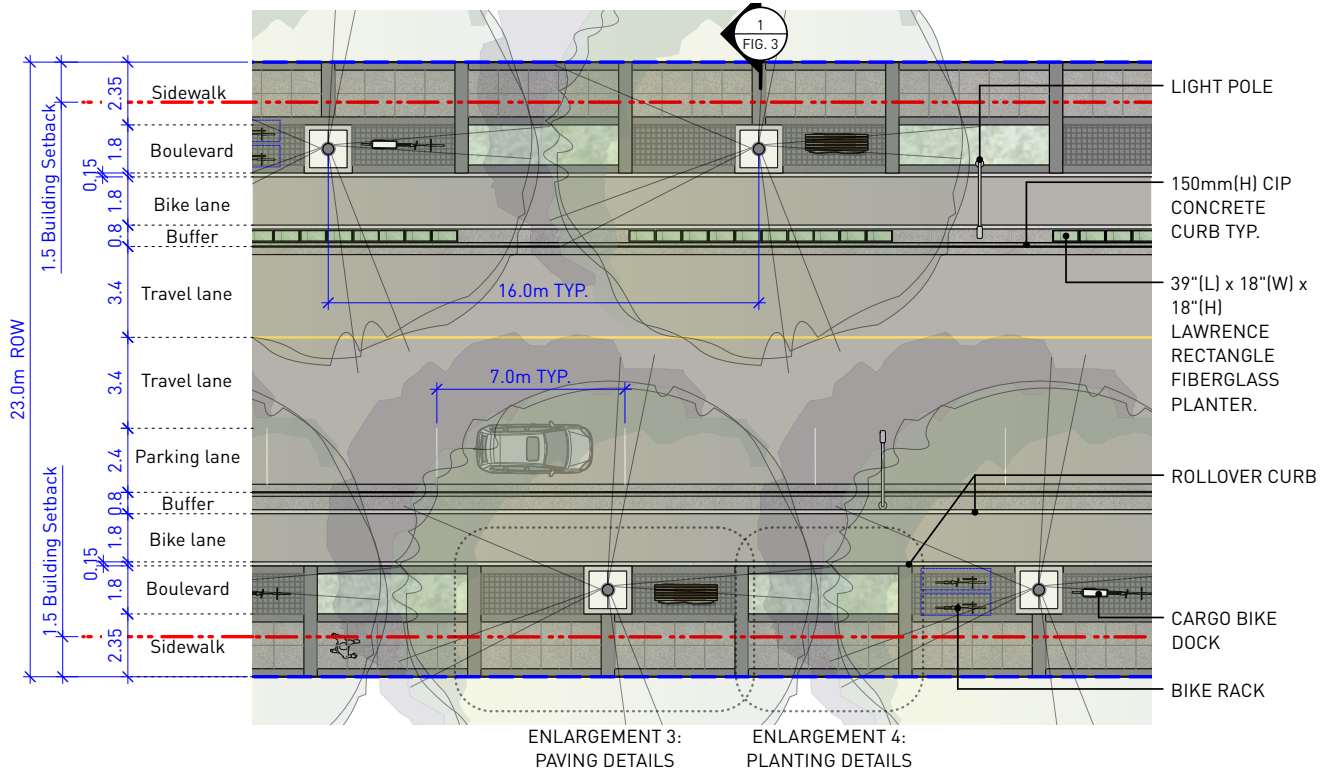


FIG. 2 ENLARGEMENT 1 - TYPICAL STREETSCAPE

Scale: 1:250

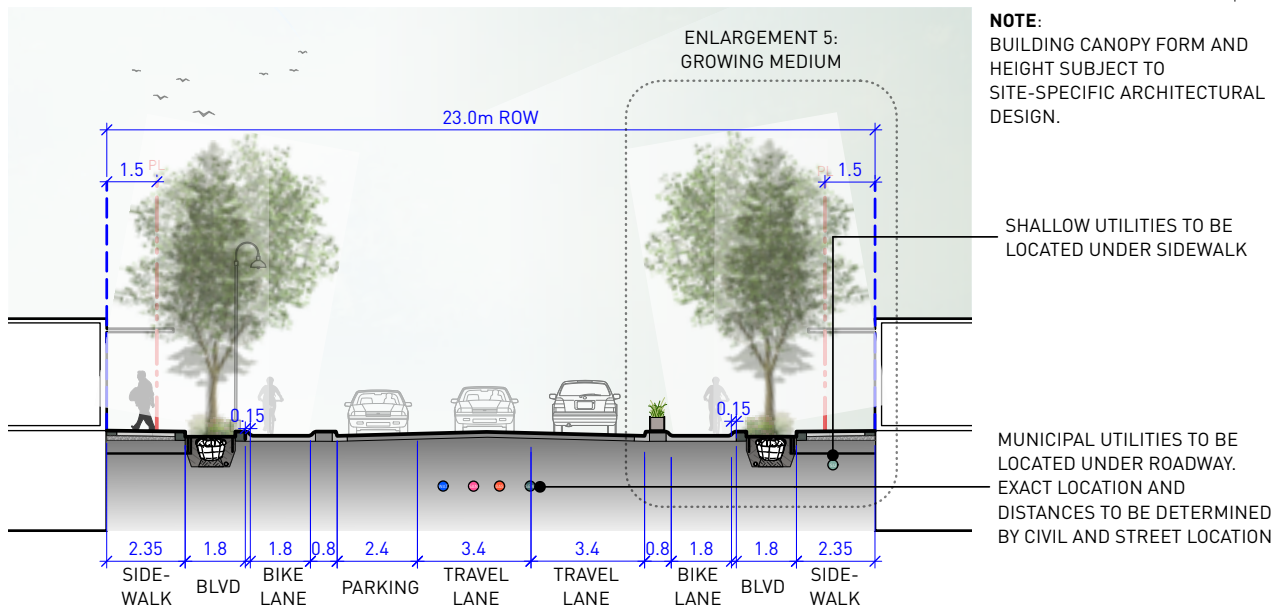


FIG. 3 SECTION 1 - TYPICAL STREETSCAPE

Scale: 1:200



PEMBERTON AVENUE

TYPICAL STREETSCAPE – EAST OF SECOND AVENUE

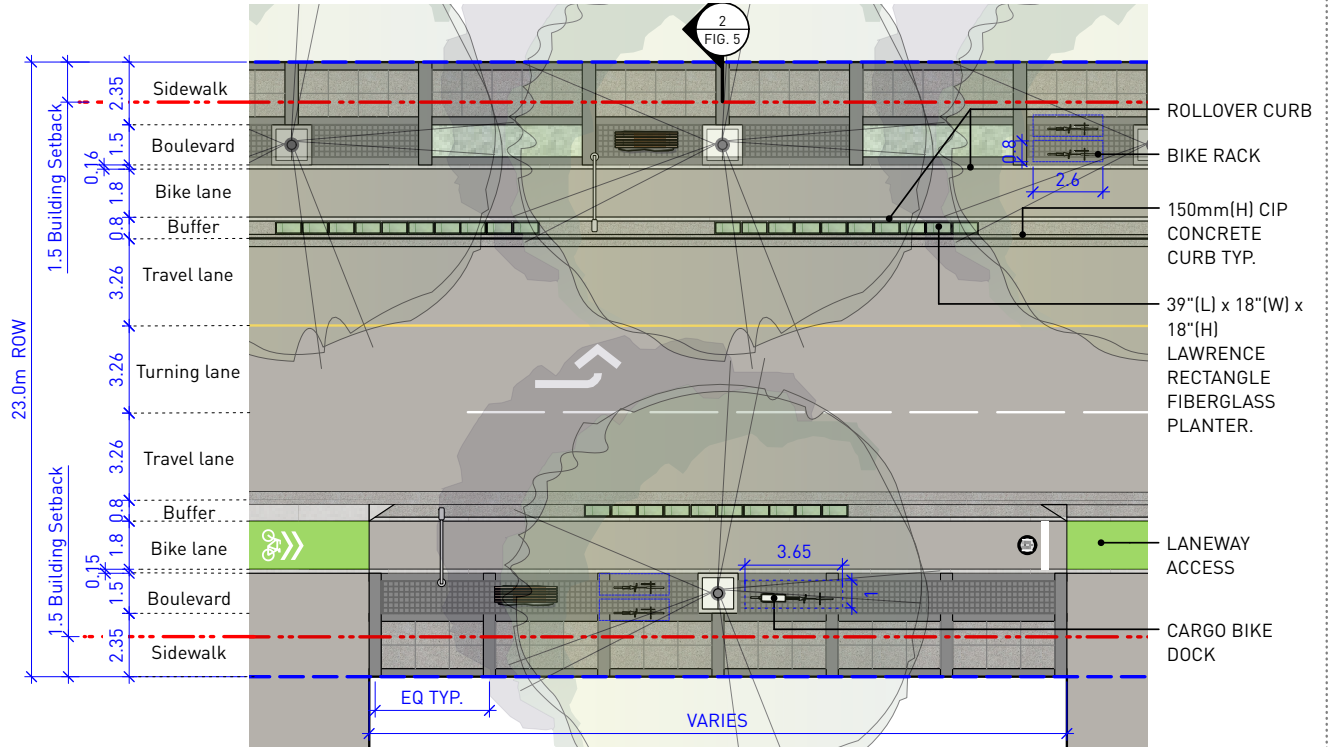
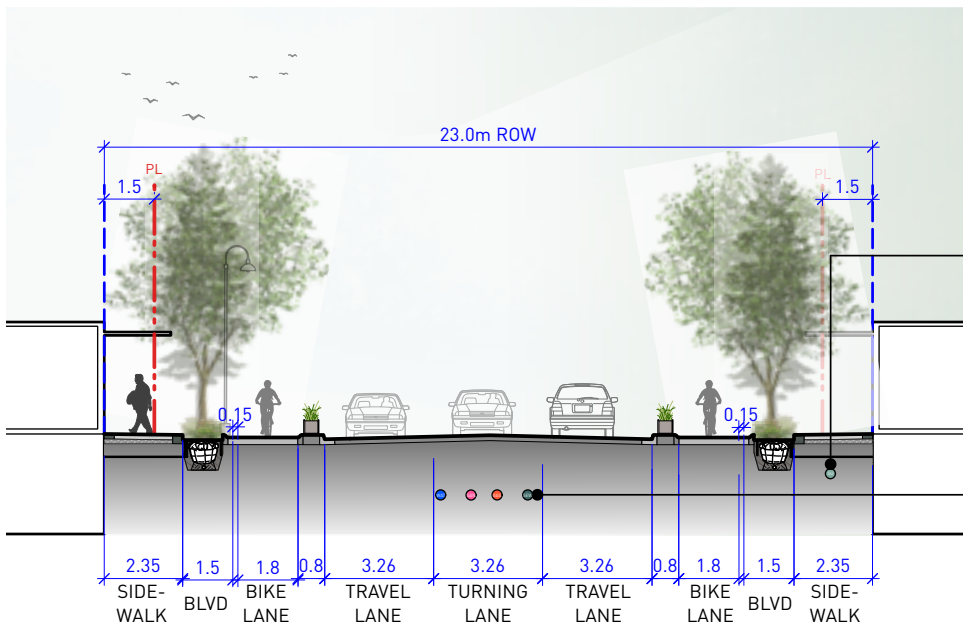


FIG. 4 ENLARGEMENT 2 - TURNING LANE AND LANEWAY ACCESS

Scale: 1:250



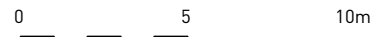
NOTE:
BUILDING CANOPY FORM AND HEIGHT SUBJECT TO SITE-SPECIFIC ARCHITECTURAL DESIGN.

SHALLOW UTILITIES TO BE LOCATED UNDER SIDEWALK

MUNICIPAL UTILITIES TO BE LOCATED UNDER ROADWAY. EXACT LOCATION AND DISTANCES TO BE DETERMINED BY CIVIL AND STREET LOCATION

FIG. 5 SECTION 2 - TYPICAL STREETSCAPE

Scale: 1:200



PEMBERTON AVENUE

GROWING MEDIUM

NOTES:

1. SOIL CELLS TO BE ARRANGED IN BLOCKS THAT SUPPORT RADIAL ROOT GROWTH FROM CENTRE OF TREE.
2. SOIL CELLS TO SUPPLEMENT GROWING MEDIUM VOLUME TO ACHIEVE MINIMUM SOIL VOLUME REQUIREMENTS IN GENERAL NOTES.
3. STRUCTURAL SOIL UNDER ROADWAY RECOMMENDED IF GEOTECHNICAL AND CIVIL ENGINEERING CONDITIONS ALLOW IN ORDER TO PROVIDE MORE SYMMETRICAL ROOT GROWTH FOR TREES. GROWING MEDIA VOLUME CONTAINED IN STRUCTURAL SOIL CAN CONTRIBUTE TO SOIL VOLUME TARGETS. USE TO BE DETERMINED ON PROJECT-BY-PROJECT BASIS.

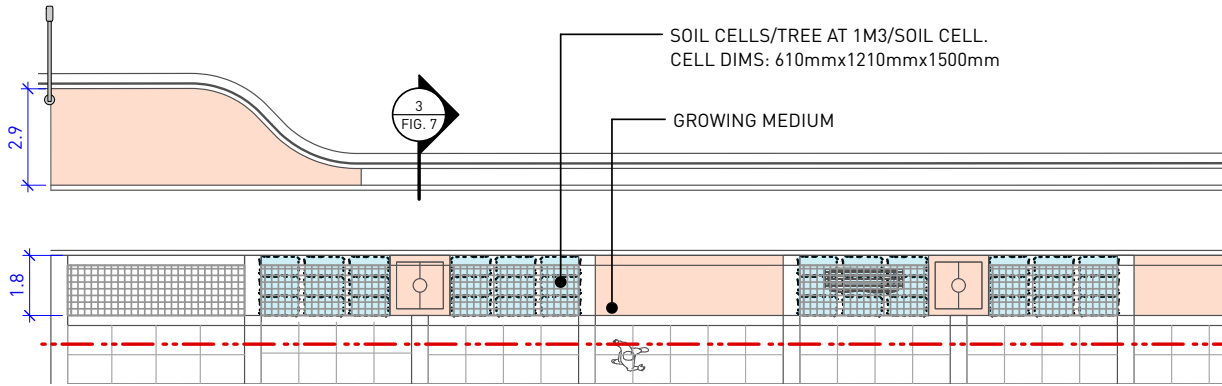
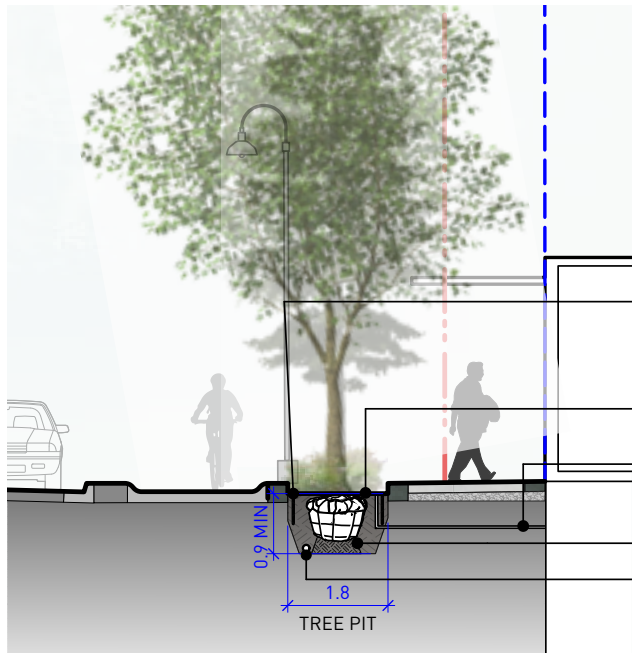


FIG. 6 ENLARGEMENT 3 - GROWING MEDIUM - WEST OF SECOND AVE.
Scale: 1:200



NOTES:

1. IRRIGATION TO BE SUPPLIED BY BUILDINGS.
2. SHRUB BEDS TO BE SUPPLIED WITH DRIP IRRIGATION, TYP.
3. TREES TO BE SUPPLIED WITH ROOT WATERING BUBBLER BY DEEP ROOT OR EQUIVALENT, TYP.

FIG. 7 ENLARGEMENT 5 - SECTION 3 - GROWING MEDIUM - WEST OF SECOND AVE.
Scale: 1:100



PEMBERTON AVENUE

PAVING

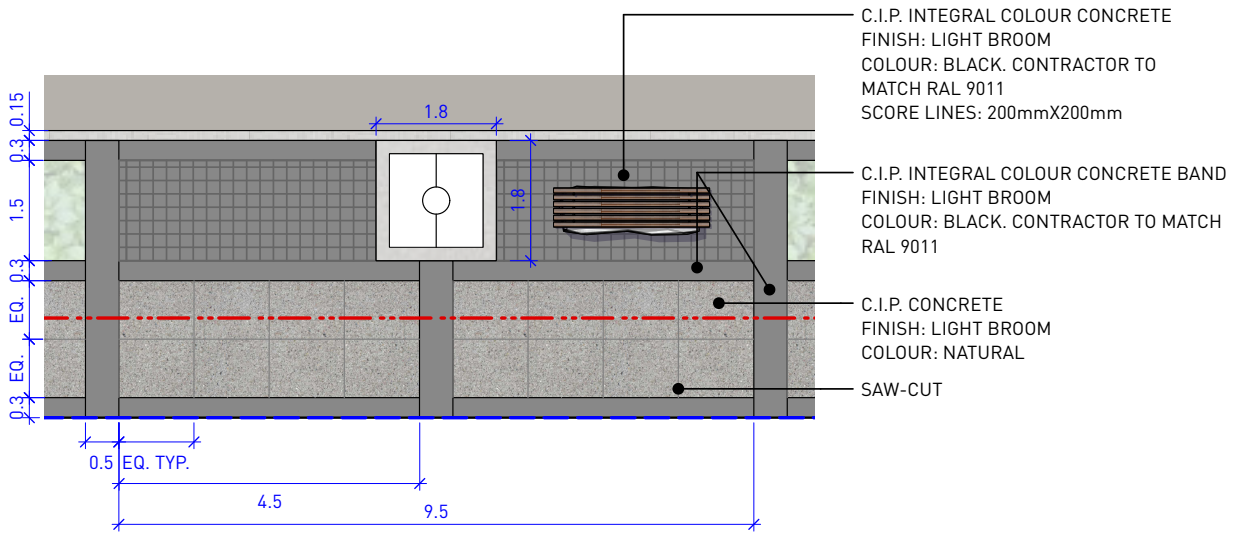


FIG. 8 ENLARGEMENT 3 - PAVING

Scale: 1:100

NOTE:

EXPANSION JOINTS TO BE 9.0M MAX BETWEEN JOINTS IN BOTH DIRECTIONS ALONG PROPERTY LINE AND AT ALL VERTICAL FACES SUCH AS CURBS AND LIGHT STANDARDS.

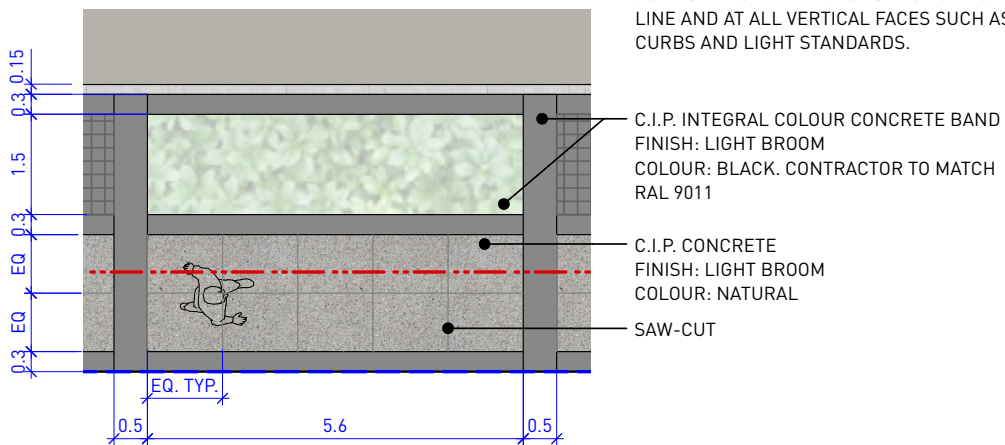


FIG. 9 ENLARGEMENT 4 - PAVING AROUND PLANTER

Scale: 1:100